

**CALIFORNIA  
COMMERCE  
CENTER**

---

AT ONTARIO

**VI. DEVELOPMENT STANDARDS AND  
CRITERIA**

## **VI. DEVELOPMENT STANDARDS AND CRITERIA**

- A. **RAIL INDUSTRIAL**
- B. **LIGHT INDUSTRIAL**
- C. **OFFICE**
- D. **COMMERCIAL/FOOD/HOTEL**
- E. **AIRPORT RELATED ALTERNATIVE**
- F. **BUILDING AND PARKING SETBACKS**
- G. **FEDERAL AVIATION ADMINISTRATION'S REGULATIONS ON CLEAR ZONE/BUILDING HEIGHTS**
- H. **SOUND ATTENUATION CRITERIA**
- I. **CRITERIA FOR DEVELOPING ADJACENT TO FREEWAYS**
- J. **AIRPORT RESTRICTIVE OVERLAYS/AIRPORT RESERVATION AREA/CLEAR ZONE/LOW EMPLOYEE INTENSITY AREA**

The following regulations and criteria establish minimum development standards for the land uses proposed in this project. These regulations shall govern all property within the California Commerce Center and shall supersede the City of Ontario Zoning Ordinance.

The Approving Agent for California Commerce Center shall review all proposed development plans prior to review and approval by the City of Ontario.

**TABLE 4  
APPLICABLE MITIGATION MEASURES**

	DUST CONTROL	NOISE	DRAINAGE	ENERGY CONSERVATION	TRAFFIC/ TRANSIT	WATER CONSERVATION	AESTHETICS	BUILDING HEIGHT	SOILS/GROUND STABILITY	RECYCLING	SAFETY/ SECURITY
	2,3,4	9,37,39, 38	15	16,17,18,19, 20	22,23, 48,49, 49A,50, 51,52, 53,56, 57,58, 59	26	31,65, 31A	33,33A	41,42,43,44, 44A	72,72A	72-79
A. Rail Industrial	X	X	X	X	X	X		X	X	X	X
B. Light Industrial	X	X	X	X	X	X	X	X	X	X	X
C. Office	X	X	X	X	X	X	X	X	X	X	X
D. Commercial/ Food/Hotel	X	X*	X	X	X	X	X	X	X	X	X

\*The Environmental Mitigation Measures listed in Section VIII-B apply to the development of projects within CCC. Please consult this chart for specifics.

A.

## EXHIBIT 38

### RAIL INDUSTRIAL

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#### PERMITTED USES

- Administrative/professional/business offices associated with on-site permitted uses
- Distribution/storage/warehousing, within an enclosed structure
- Interim agricultural production
- Manufacturing/assembly, testing/repair
- Research/development/laboratories
- Wholesale and limited retail sales of products manufactured or warehoused on-site

#### LIMITED USES

The following uses require review by the City Planner prior to approval of a site plan or business license. To approve the use at the proposed location, the City Planner must determine that parking, access, and/or any other factors associated with the use or the location will be adequately resolved.

- Auto/truck services

All other uses are prohibited unless a finding is made by the Planning Commission that the use is similar to and no more objectionable than a permitted or a limited use.

MINIMUM PARCEL SIZE	2.5 acres
MAXIMUM BUILDING HEIGHT	Refer to Federal Aviation Administration Regulations as shown in Section VI.G
MINIMUM BUILDING SETBACKS (Measured from property line)	Refer to Building Setbacks Summary, Exhibit 48
MINIMUM PARKING SETBACKS	Refer to Parking Setbacks Summary, Exhibit 49
PARKING REGULATIONS (per City of Ontario Parking Standards)	Refer to Appendix B

#### LANDSCAPE

##### MINIMUM LANDSCAPE REQUIREMENTS

• Minimum landscape coverage	Not applicable
• Building front and exterior side setbacks	100%
• Parking front and exterior side setbacks	100%
• Front and exterior side building elevations softened by minimum landscaped area	10'
• Interior side building setback (interior parcels, in front of concrete screenwall)	5'
• In front of screen walls (see Exhibit 39)	6'
• All parking visible from any public street must be shielded by bermed mounding planted with trees, shrubs, and grass per Master Streetscape Plan	Front and exterior setbacks
• Maximum 10 cars between finger type planters, minimum 6' wide from parking areas (in front of concrete screenwall)	Front, exterior side and interior side (refer to Conceptual Site Plan)
• All landscaped areas to be delineated with minimum 6" concrete curb	100%

A.

EXHIBIT 38 (Continued)

RAIL INDUSTRIAL

LOADING AREAS

1. Loading areas shall be designed to provide for backing and maneuvering on-site and not from a public street.
2. Loading areas shall not encroach into building setbacks.
3. All loading areas shall be screened from adjacent parcels and streets.
4. Buildings shall be designed per the conceptual site plans shown in Exhibits 39, 40, 42, 43 which show loading areas primarily located to the side and rear of the building.
5. Where loading doors front a public street, roll-up doors and openings in the screen wall shall be positioned such that the doors are not visible from the street.
6. All loading areas fronting a public street shall be screened by a combination of screen walls, ornamental landscaping, and/or portions of the building such that the roll-up doors are not visible from the street.
7. Loading areas and doors not fronting a public street shall be screened from view of the public street by concrete wing walls with redwood slatted gate (or equal) and ornamental landscaping.
8. Loading doors fronting a public street shall not be closer than 70' from property line.
9. All screenwalls and wing walls shall be a maximum of 12' in height.
10. A sight-line analysis shall be required with all development applications, and shall show that all roll-up doors are screened from view from adjoining parcels and public streets (see diagram on following page as an example of acceptable sight-line analysis - Exhibit 40A).

OUTDOOR STORAGE

- No outdoor storage shall be permitted unless adequately screened by an opaque material approved by California Commerce Center's Approving Agent.
- All storage screening shall be a minimum of 8' in height and no material shall be stored higher than 8'.
- All storage areas fronting a public street shall be screened by a concrete screen wall and ornamental landscaping.
- Location of outdoor storage areas shall be shown on the development site plan, and shall be subject to approval by California Commerce Center's Approving Agent and the City of Ontario Development Advisory Board.

EXTERIOR BUILDING MATERIALS

- All building improvements, with the exception of trim and minor architectural features, shall be constructed of masonry, concrete, glass, or other material approved by California Commerce Center's Approving Agent (no precision block).
- All exterior walls shall be painted or suitably treated.

ROOFING AND ROOFTOP EQUIPMENT

- Opaque screening approved by California Commerce Center's Approving Agent, shall be provided to conceal all rooftop equipment.
- Unless roofing materials are a part of the design element (shingles, tile, etc.), the ridge line elevation shall not exceed the parapet elevation.
- All mechanical equipment on top of the roof shall be painted to blend with the building and roofing materials.

SIGNAGE

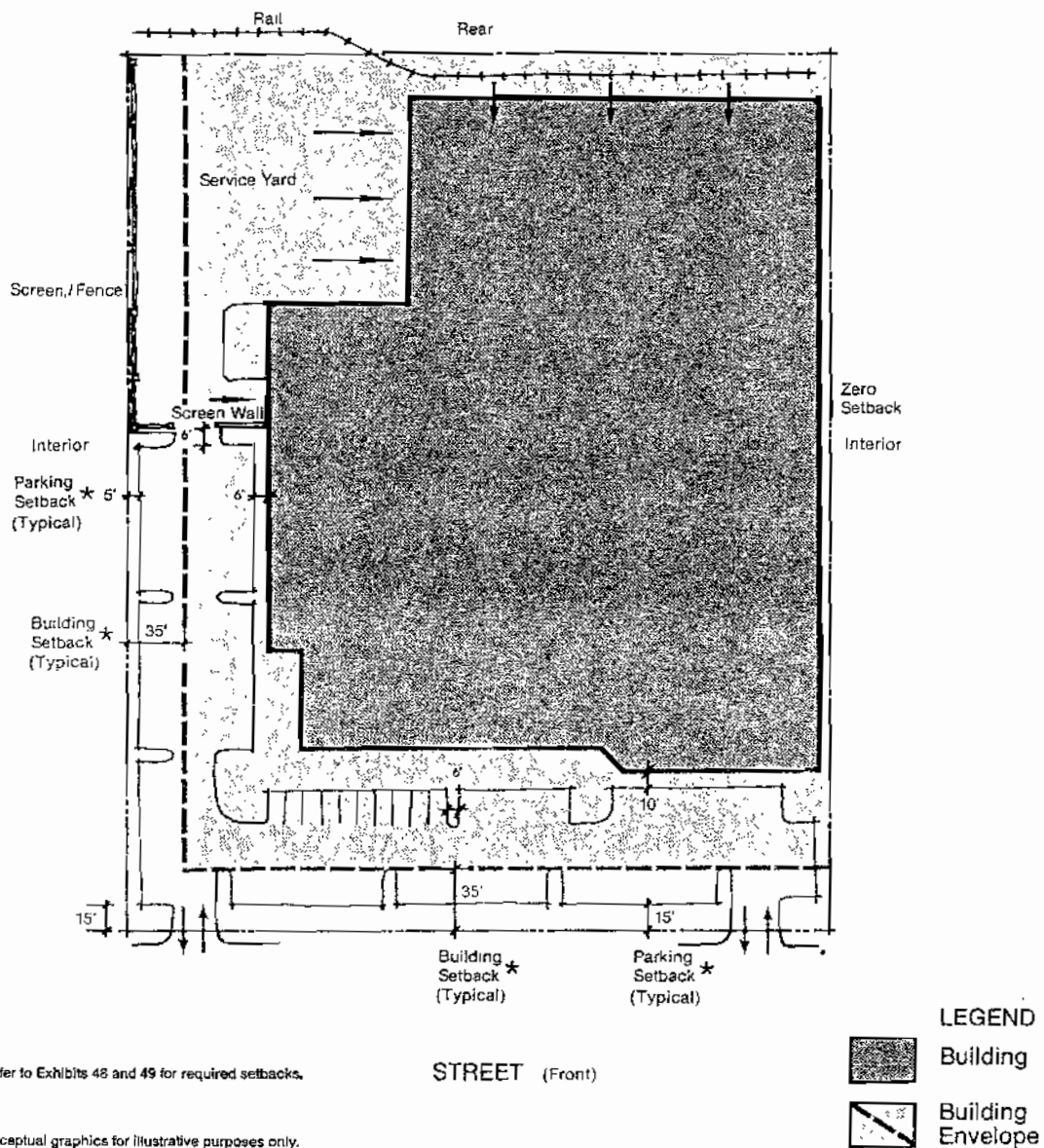
- All signs shall be subject to the provisions of the Master Signage Plan and will require the approval of California Commerce Center's Approving Agent and the City of Ontario (refer to Section VII. A and Signage and Graphics Criteria, Exhibit 56).

A.

EXHIBIT 39

RAIL INDUSTRIAL  
(Interior Parcel)

Conceptual Site Plan

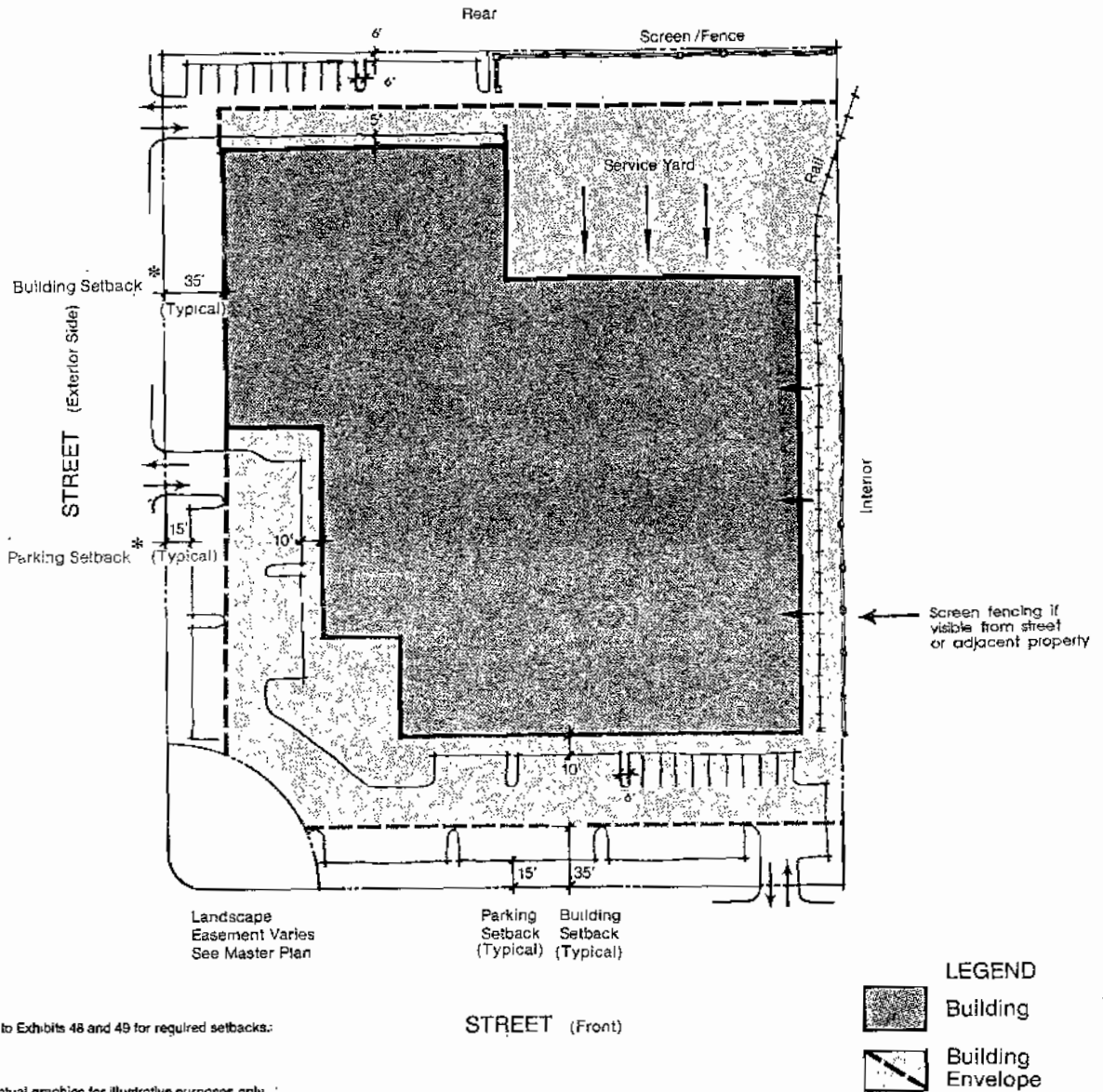


A.

EXHIBIT 40

RAIL INDUSTRIAL  
(Corner Parcel)

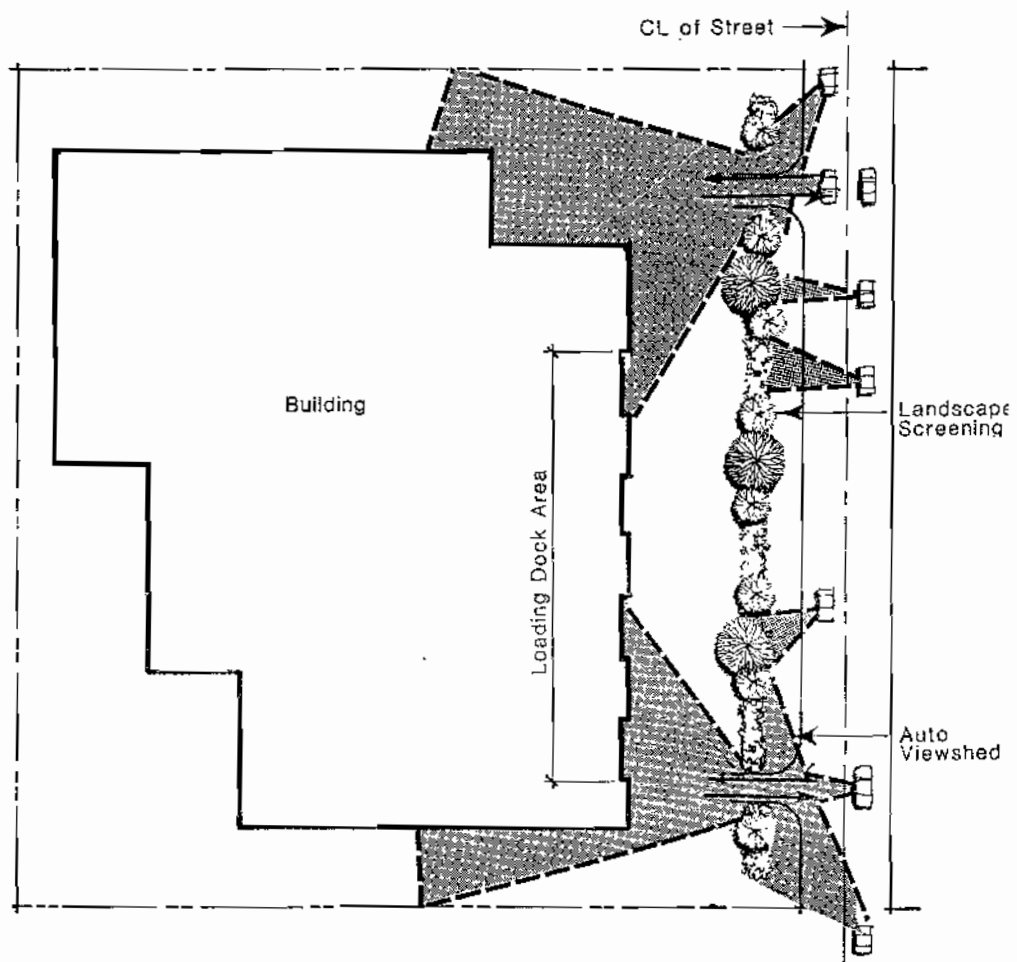
Conceptual Site Plan



A.

EXHIBIT 40-A

SIGHT LINE ANALYSIS





## LIGHT INDUSTRIAL

### PERMITTED USES

- Administrative/professional/general business offices in conjunction with an on-site permitted use
- Airport related uses such as Air Cargo and airline food service facilities (refer to Exhibit 47C, Airport Related Alternative)
- Distribution/storage/warehousing, within an enclosed structure
- Manufacturing/assembly/testing/repair
- Research/development/laboratories
- Retail auto center east of I-15 Freeway and at the northeast corner of Jurupa Street and Woodruff Way, including ancillary uses, e.g., auto repair
- Service industries including, but not limited to: public utilities, printing/lithography, auto rental agencies
- Wholesale/retail sales of products manufactured or warehoused on-site

### LIMITED USES

The following uses require review by the City Planner prior to approval of a site plan or business license. To approve the use at the proposed location, the City Planner must determine that parking, access, and/or any other factors associated with the use or the location will be adequately resolved.

- Administrative/professional/general business offices
- Financial institutions
- Medical and health care clinics
- Recreation facilities
- Restaurants, sandwich shops, delicatessens, donut shops, etc.

All other uses are prohibited unless a finding is made by the Planning Commission that the use is similar to and no more objectionable than a permitted or a limited use.

#### MINIMUM PARCEL SIZE 1.0 ACRE EXCEPT AS FOLLOWS:

A subdivision with less than one (1) acre parcels may be permitted subject to the following conditions:

- a. Minimum site area shall be twenty (20) acres.
- b. The 20-acre site shall be masterplanned as a unit subject to approval by the City of Ontario Development Advisory Board.
- c. Construction must be underway, or building permits must be issued, for those buildings located on the proposed subdivision site.
- d. Minimum parcel size under this provision shall be one half (0.5) acre.

#### MAXIMUM BUILDING HEIGHT

Refer to Federal Aviation Administrative Regulations as shown in Section VI.G

#### MINIMUM BUILDING SETBACKS (Measured from property line)

Refer to Building Setback Summary, Exhibit 48

#### MINIMUM PARKING SETBACKS

Refer to Parking Setback Summary Exhibit 49

#### PARKING REQUIREMENT (per City of Ontario Parking Standards)

Refer to Appendix B

## LIGHT INDUSTRIAL

## LANDSCAPE

## MINIMUM LANDSCAPE REQUIREMENTS

- |  |                              |
|--|------------------------------|
| • Minimum landscape coverage   | Not applicable               |
| • Building front and exterior side setbacks  | 100%                         |
| • Parking front and exterior side setbacks   | 100%                         |
| • Front and exterior side building elevations softened by minimum landscaped area  | 10'                          |
| • Interior side parking setback (in front of concrete screenwall)  | 10' (see exhibits 42 and 43) |
| • All parking visible from any public street must be shielded by bermed mounding planted with trees, shrubs, and grass per Master Streetscape Plan | Front and exterior setbacks  |
| • Maximum 10 cars between finger type planters, minimum 6' wide from parking areas   | 100%                         |
| • All landscaped areas to be delineated with minimum 8" concrete curb  |                              |

## LOADING AREAS

1. Loading areas shall be designed to provide for backing and maneuvering on-site and not from a public street.
2. Loading areas shall not encroach into building setbacks.
3. All loading areas shall be screened from adjacent parcels and streets.
4. Buildings shall be designed per the conceptual site plans shown in Exhibits 39, 40, 42, 43 which show loading areas primarily located to the side and rear of the building.
5. Where loading doors front a public street, roll-up doors and openings in the screen wall shall be positioned such that the doors are not visible from the street.
6. All loading areas fronting a public street shall be screened by a combination of screen walls, ornamental landscaping, and/or portions of the building such that the roll-up doors are not visible from the street.
7. Loading areas and doors not fronting a public street shall be screened from view of the public street by concrete wing walls with redwood slatted gate, or equal.
8. Loading doors fronting a public street shall not be closer than 70' from property line.
9. All screenwalls and wing walls shall be a maximum of 12' in height.
10. A sight-line analysis shall be required with all development applications, and shall show that all roll-up doors are screened from view from adjoining parcels and public streets (see diagram in Exhibit 40A).

## OUTDOOR STORAGE

- No outdoor storage shall be permitted unless adequately screened by an opaque material approved by California Commerce Center's Approving Agent.
- All storage screening shall be a minimum of 8' in height and no material shall be stored higher than 8'.
- All storage areas fronting a public street shall be screened by a concrete screen wall and ornamental landscaping.
- Location of outdoor storage areas shall be shown on the development site plan, and shall be subject to approval by California Commerce Center's Approving Agent and the City of Ontario Development Advisory Board.

## EXTERIOR BUILDING MATERIALS

- All building improvements, with the exception of trim and minor architectural features, shall be constructed of masonry, concrete, glass, or other material approved by California Commerce Center's Approving Agent (no precision block).
- All exterior walls shall be painted or suitably treated.

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**LIGHT INDUSTRIAL**

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**ROOFING AND ROOFTOP EQUIPMENT**

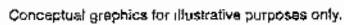
- Opaque screening approved by California Commerce Center's Approving Agent shall be provided to conceal all rooftop equipment.
- Unless roofing materials are a part of the design element (shingles, tile, etc.), the ridge line elevation shall not exceed the parapet elevation.
- All mechanical equipment on top of the roof shall be painted to blend with the building and roofing materials.

**SIGNAGE**

- All signs shall be subject to the provisions of the Master Signage Plan and will require the approval of California Commerce Center's Approving Agent and the City of Ontario, (refer to Section VII.A and Signage and Graphics Criteria, Exhibit 56).

EXHIBIT 42

## Conceptual Site Plan

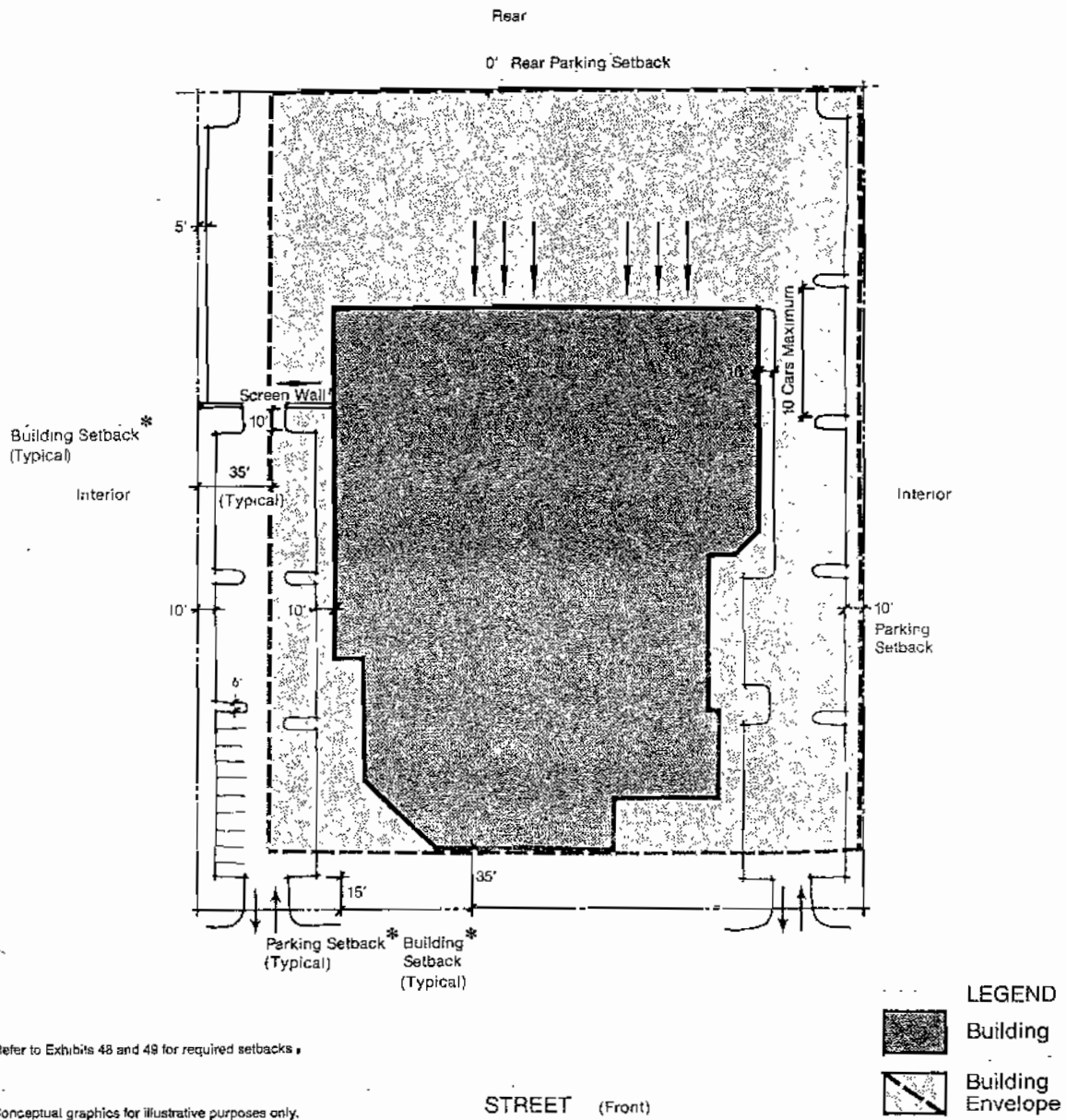


B.

EXHIBIT 43

LIGHT INDUSTRIAL  
(Interior Parcel)

Conceptual Site Plan



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**PERMITTED USES**

- Administrative/professional/general business offices
- Commercial and office services, e.g. print shops/retail office supplies
- Financial institutions
- Interim agricultural uses
- Medical/health care clinics
- Research/development/laboratories
- Restaurants/sandwich shops, delicatessens, donut shops, etc.

All other uses are prohibited unless a finding is made by the Planning Commission that the use is similar to and no more objectionable than a permitted use.

MINIMUM PARCEL SIZE	Not Applicable
MAXIMUM BUILDING HEIGHT	Refer to Federal Aviation Administration Regulations as shown in Section VI.G
MINIMUM BUILDING SETBACKS (Measured from property line)	Refer to Building Setback Summary, Exhibit 48
MINIMUM PARKING SETBACKS	Refer to Parking Setback Summary, Exhibit 49
PARKING REQUIREMENTS (per City of Ontario Parking Standards)	Refer to Appendix B

**LANDSCAPE****MINIMUM LANDSCAPE REQUIREMENTS**

- |   |      |
|---|------|
| • Minimum parcel landscape coverage   | 15%  |
| • Building front and exterior, interior and rear side setbacks  | 100% |
| • Parking front and exterior, interior and rear side setbacks   | 100% |
| • Elevations softened by minimum landscaped area  | 15'  |
| • All parking visible from any public street must be shielded by bermed mounding planted with trees, shrubs and grass per Master Streetscape Plan | 100% |
| • Maximum 10 cars between finger type planters minimum 6' wide for parking areas  | 100% |
| • All landscaped areas to be delineated with minimum 6" concrete curb.  | 100% |

**LOADING AREAS**

- Loading areas shall be designed to provide for backing and maneuvering on-site and not from a public street.
- Loading areas shall not be visible from street or on-site passenger circulation drives and shall be screened from adjacent parcels.

**OUTDOOR STORAGE**

- No outdoor storage shall be permitted unless adequately screened by an opaque material approved by California Commerce Center's Approving Agent.

- 
- All storage areas fronting a public street shall be screened by a concrete screen wall and ornamental landscaping.
  - Location of outdoor storage areas shall be shown on the development site plan, and shall be subject to approval by California Commerce Center's Approving Agent and the City of Ontario Development Advisory Board.

**EXTERIOR BUILDING MATERIALS**

- All building improvements, with the exception of trim and minor architectural features shall be constructed of masonry, concrete, glass or other material approved by California Commerce Center's Approving Agent (no precision block).
- All exterior walls shall be painted or suitably treated.

**ROOFING AND ROOFTOP EQUIPMENT**

- Opaque screening approved by California Commerce Center's Approving Agent, shall be provided to conceal all rooftop equipment.
- Unless roofing materials are a part of the design element (shingles, tile, etc.), the ridge line elevation shall not exceed the parapet elevation.
- All mechanical equipment on top of the roof shall be painted to blend with the building and roofing materials.

**SIGNAGE**

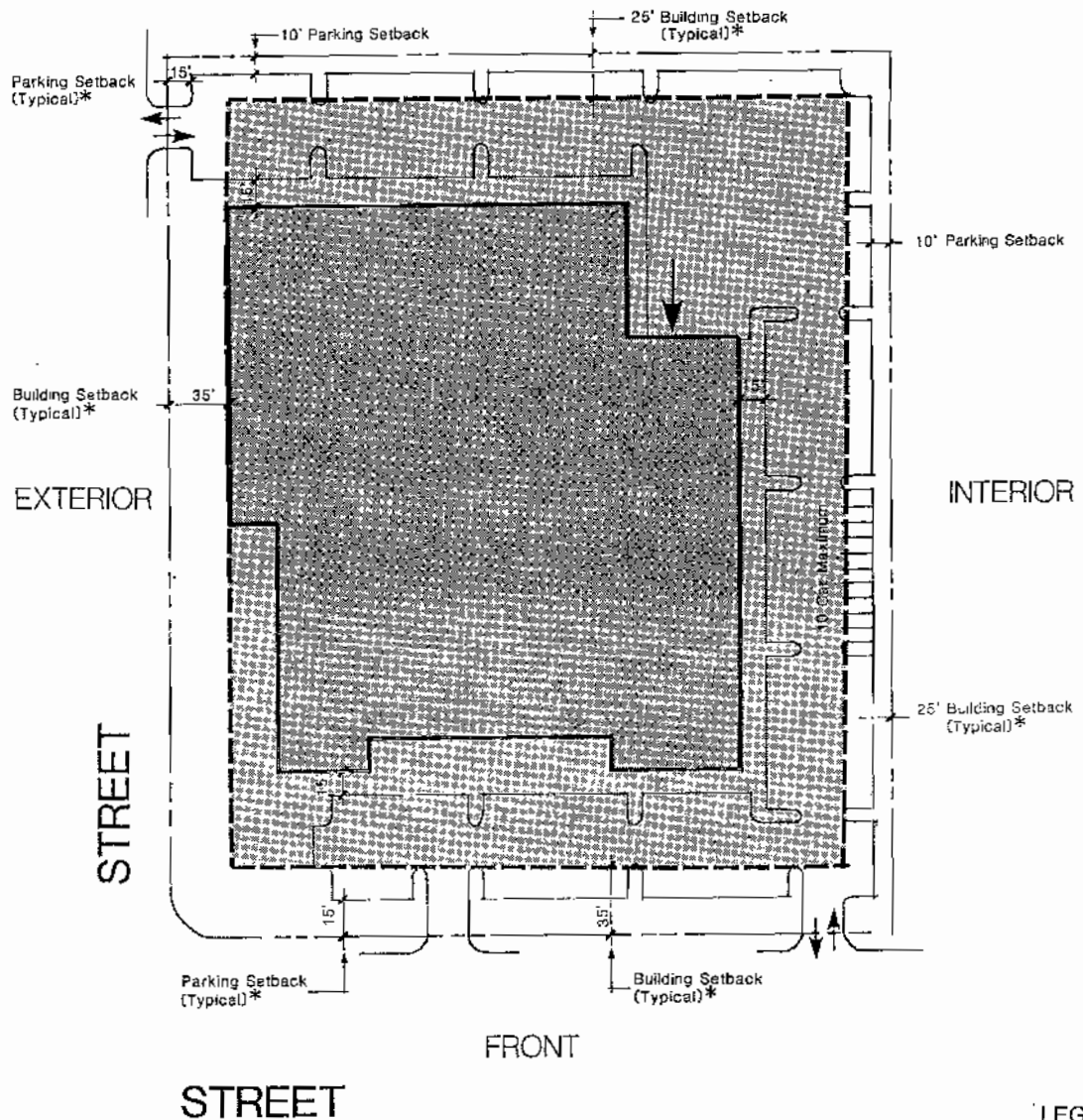
- All signs shall be subject to the provisions of the Master Signage Plan and will require the approval of California Commerce Center's Approving Agent and the City of Ontario, (refer to Section VII.A and Signage and Graphics Criteria, Exhibit 56.)

C.

EXHIBIT 45

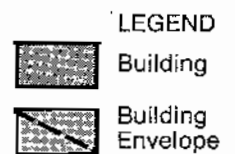
OFFICE  
(Corner Parcel)

# Conceptual Site Plan



\*Refer to Exhibits 48 and 49 for required setbacks.

Conceptual graphics for illustrative purposes only.



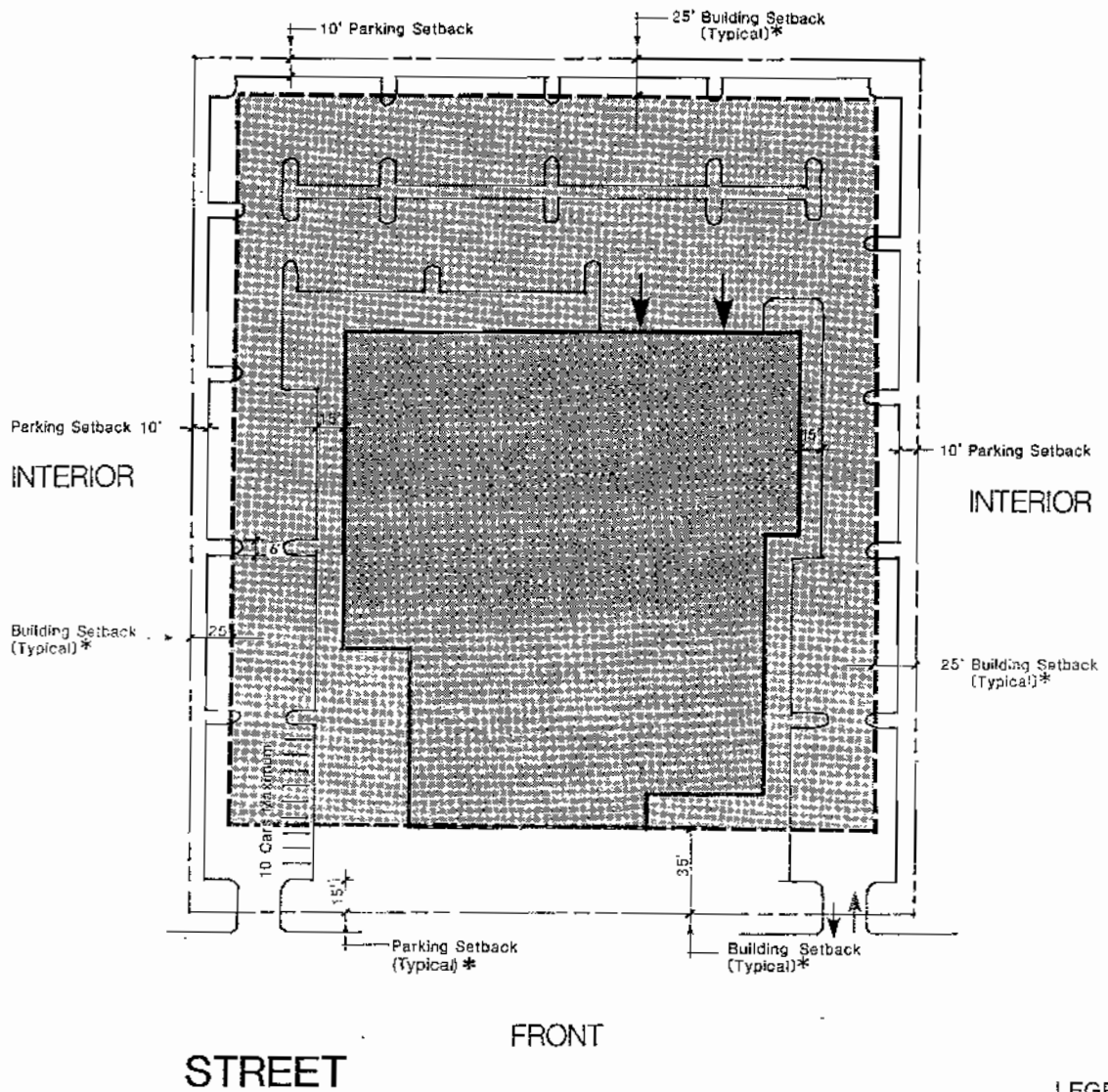


C.

EXHIBIT 46

OFFICE  
(Interior Parcel)

# Conceptual Site Plan



\*Refer to Exhibits 48 and 49 for required setbacks.

Conceptual graphics for illustrative purposes only.

- LEGEND -
- Building
  - Building Envelope

## COMMERCIAL/FOOD/HOTEL

## PERMITTED USES

- Administrative/professional/general business offices
- Automobile rental agencies
- Auto service
- Financial institutions
- Health and recreation facilities
- Hotels/motels/hometels
- Interim agricultural production
- Medical and health care facilities
- Restaurants, sandwich shops, delicatessens, donut shops, etc.
- Retail sales (excluding auto and truck sales) and office services, e.g. print shops, courier services, etc.

## LIMITED USES

The following uses require review by the City Planner prior to approval of a site plan or business license. To approve the use at the proposed location, the City Planner must determine that parking, access, and/or any other factors associated with the use or the location will be adequately resolved.

- Child day care centers

All other uses are prohibited unless a finding is made by the Planning Commission that the use is similar to and no more objectionable than a permitted or a limited use.

MINIMUM PARCEL SIZE	Not Applicable
MAXIMUM BUILDING HEIGHT	Refer to Federal Aviation Administration Regulations as shown in Section VI.G
MINIMUM BUILDING SETBACKS (Measured from property line)	Refer to Building Setback Summary, Exhibit 48
MINIMUM PARKING SETBACKS	Refer to Parking Setback Summary, Exhibit 49
PARKING REQUIREMENTS (per City of Ontario Parking Standards)	Refer to Appendix B

## LANDSCAPE

## MINIMUM LANDSCAPE REQUIREMENTS

- |  |      |
|--|------|
| • Minimum landscape coverage   | 15%  |
| • Building front and exterior, interior and rear side setbacks   | 100% |
| • Parking front and exterior, interior and rear side setbacks  | 100% |
| • Elevations softened by minimum landscaped area   | 15'  |
| • All parking visible from any public street must be shielded by bermed mounding planted with trees, shrubs, and grass per Master Streetscape Plan | 100% |
| • Maximum 10 cars between finger type planters, minimum 6' wide for parking areas  | 100% |
| • All landscaped areas to be delineated with minimum 6" concrete curb.   | 100% |

**COMMERCIAL/FOOD/HOTEL**

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**LOADING AREAS**

- Loading areas shall be designed to provide for backing and maneuvering on-site and not from a public street.
- Loading areas shall not be visible from street or on-site passenger circulation drives and shall be screened from adjacent parcels.

**OUTDOOR STORAGE**

- No outdoor storage shall be permitted.

**EXTERIOR BUILDING MATERIALS**

- All building improvements, with the exception of trim and minor architectural features, shall be constructed of masonry, concrete, glass, or other material approved by California Commerce Center's Approving Agent (no precision block).
- All exterior walls shall be painted or suitably treated.

**ROOFING AND ROOFTOP EQUIPMENT**

- Opaque screening approved by California Commerce Center's Approving Agent, shall be provided to conceal all rooftop equipment.
- Unless roofing materials are a part of the design element (shingles, tile, etc.), the ridge line elevation shall not exceed the parapet elevation.
- All mechanical equipment on top of the roof shall be painted to blend with the building and roofing materials.

**SIGNAGE**

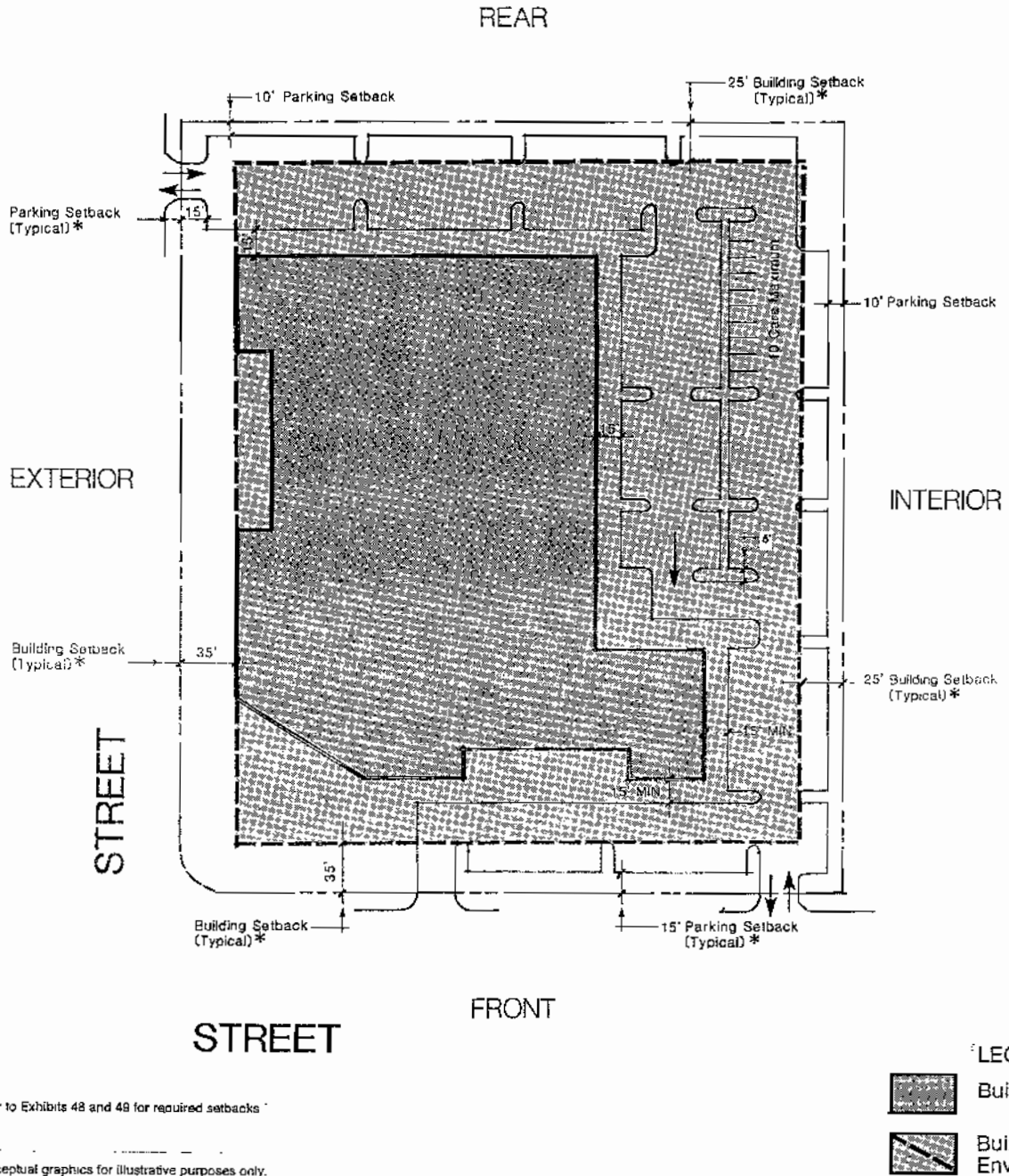
- All signs shall be subject to the provisions of the Master Signage Plan and will require the approval of California Commerce Center's Approving Agent and the City of Ontario, (refer to Section VII.A and Signage and Graphics Criteria, Exhibit 56).

D.

EXHIBIT 47-A

COMMERCIAL/FOOD/HOTEL  
(Corner Parcel)

Conceptual Site Plan

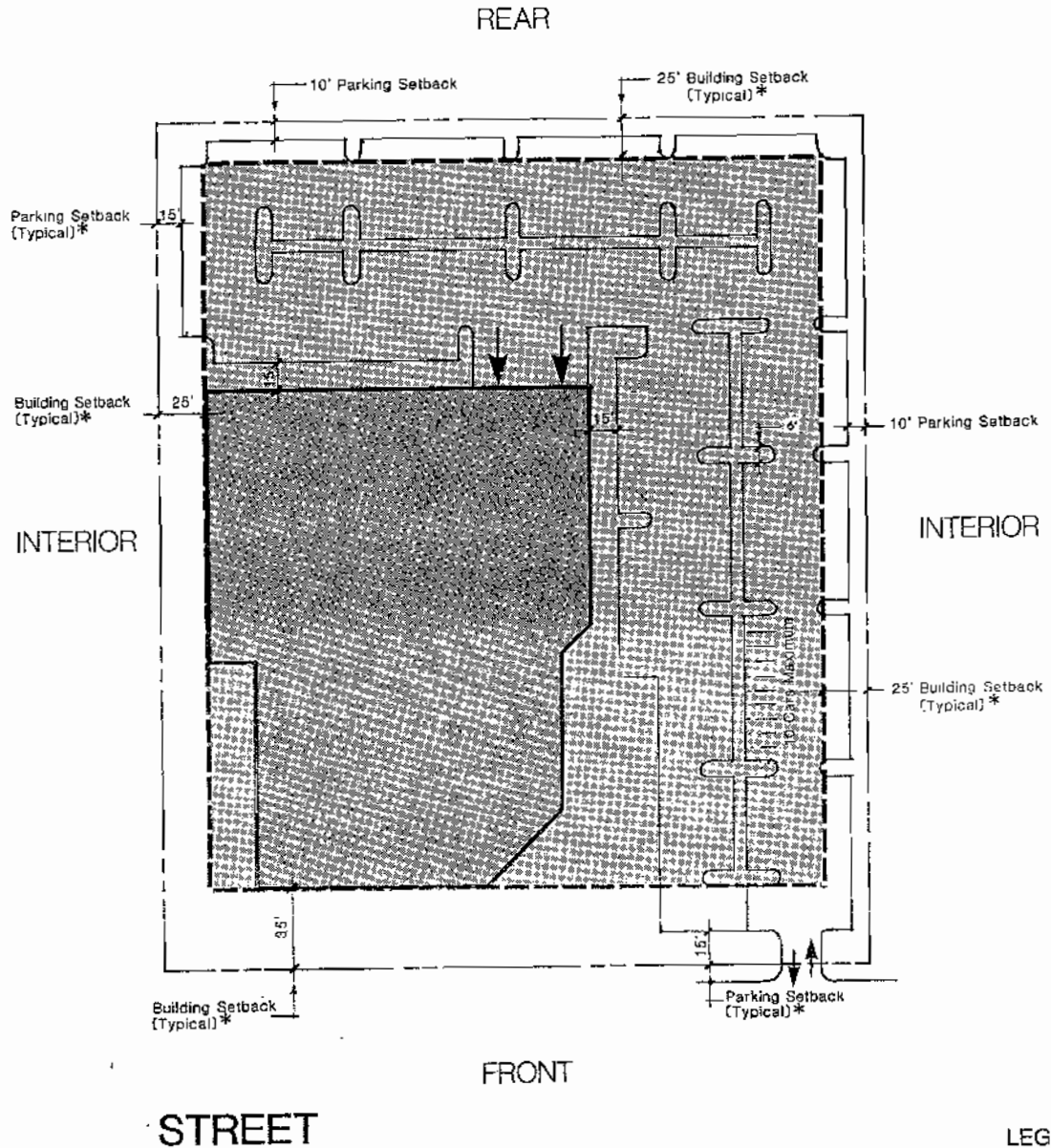


D.

EXHIBIT 47-B

COMMERCIAL/FOOD/HOTEL  
(Interior Parcel)

Conceptual Site Plan



\*Refer to Exhibits 48 and 49 for required setbacks.

Conceptual graphics for illustrative purposes only.

**AIRPORT RELATED ALTERNATIVE**

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**PERMITTED USES**

- Potential taxiway access to Ontario International Airport
- Aircraft manufacturing and service.
- Aircraft storage
- Air cargo
- Airline food service
- Executive air terminals
- General aviation facilities and related uses

**MINIMUM PARCEL SIZE****BUILDING COVERAGE, HEIGHT & SETBACKS****MAXIMUM BUILDING COVERAGE****MAXIMUM BUILDING HEIGHT****MINIMUM BUILDING SETBACKS (measured from property line)**

- Front
- Exterior Side (corner parcels)
- Interior Side (1 side only)
- Rear (interior parcels)
- All Buildings over 35' in height
- All Buildings over 150' in length (length determined by its dimension parallel to street)

**PARKING****MINIMUM PARKING SETBACKS**

- Front
- Exterior Side (corner parcels)
- Interior Sides (front of Screenwall)
- Rear (interior parcels)

**PARKING REQUIREMENTS****MINIMUM PARKING REQUIREMENTS**

- Aircraft manufacturing
- Service area
- Storage/Air cargo areas
- Airline food service area
- Executive Air Terminals
- General Aviation Facilities
- Related Uses

**LANDSCAPE****MINIMUM LANDSCAPE REQUIREMENTS****LOADING AREAS****OUTDOOR STORAGE****EXTERIOR BUILDING MATERIALS****ROOFING AND ROOFTOP EQUIPMENT****SIGNAGE**

Note: If this alternative is implemented, development standards will be prepared during the "minor revision process", based on a more specific development design and prior to submittal of a specific development plan. That which is provided above is a general outline for use in developing any future standards.

## BUILDING SETBACK SUMMARY

	RAIL INDUSTRIAL	LIGHT INDUSTRIAL	OFFICE	COMMERCIAL/ FOOD/HOTEL
<u>Jurupa Street, Commerce Parkway</u> (from Airport Drive extending approx. 700' south of Jurupa) <u>and Airport Drive</u> (west of Milliken)				
Front	40'	40'	40'	40'
Exterior Side (Corner parcels)	40'	40'	40'	40'
Interior Side (One side only)	-0-	-0-	35'	35'
Interior Side (Other side)	35'	10'	35'	35'
Rear	-0-	-0-	35'	35'
<u>Milliken Avenue, Haven Avenue</u>				
Front	45'	45'	45'	45'
Exterior Side (Corner parcels)	32'	32'	32'	32'
Interior Side (One side only)	-0-	-0-	35'	35'
Interior Side (Other side)	35'	10'	35'	35'
Rear	-0-	-0-	35'	35'
<u>Airport Drive</u> (east of Milliken Avenue)				
Front	23'	23'	23'	23'
Exterior Side (Corner parcels)	40'	40'	40'	40'
Interior Side (One side only)	-0-	-0-	35'	35'
Interior Side (Other side)	35'	10'	35'	35'
Rear	-0-	-0-	35'	35'
<u>All Other Streets</u>				
Front	35'	35'	35'	35'
Exterior Side (Corner parcels)	35'	35'	35'	35'
Interior Side (One side only)	-0-	-0-	25'	25'
Interior Side (Other side)	35'	10'	25'	25'
Rear	-0-	-0-	25'	25'
All Buildings Over 35' In Height	1' Front Setback per 1' of Height above 35' to Maximum 70'			
All Buildings Over 150' In Length	1' Front Setback per 10' of Length over 150' to Maximum 50'			

## Notes:

1. All setbacks are measured from the property line.
2. All rear setbacks for rail served parcels shall be subject to final approval of California Commerce Center's Approving Agent based upon the final design standards and guidelines of the CC&R's.
3. Rear and Interior side building setbacks shall be subject to the following requirements pursuant to section 506(b) of the California Uniform Building Code:
  - (a)
    - (i) Either maintain a minimum of 60 feet building setback when adjacent parcels are undeveloped; or
    - (ii) Maintain an overall 60 feet building separation between the proposed building and existing buildings on adjacent parcels; or
    - (iii) Maintain a minimum of 30 feet building setback when buildings on adjacent parcels are setback a minimum of 30 feet.
  - (b) Any other Rear and Interior side setbacks may be as indicated in the Specific Plan Standards so long as proposed buildings meet all other Uniform Building Code Standards.

F.

EXHIBIT 49

## PARKING SETBACK SUMMARY

	RAIL INDUSTRIAL	LIGHT INDUSTRIAL	OFFICE	COMMERCIAL/ FOOD HOTEL
<u>Jurupa Street, Commerce Parkway</u> (between Airport Drive and Peashtree Street) and <u>Airport Drive</u> (west of Milliken Avenue)				
Front	20'	20'	20'	20'
Exterior Side (Corner Parcels)	20'	20'	20'	20'
Interior Sides	5'	10'	10'	10'
Rear	-0-	-0-	10'	10'
<u>Milliken Avenue</u> <u>Haven Avenue</u>				
Front	12'	12'	12'	12'
Exterior Side (Corner Parcels)	12'	12'	12'	12'
Interior Sides	5'	10'	10'	10'
Rear	-0-	-0-	10'	10'
<u>Airport Drive</u> (east of Milliken Avenue)				
Front	3'	3'	3'	3'
Exterior Side (Corner Parcels)	3'	3'	3'	3'
Interior Sides	5'	10'	10'	10'
Rear	-0-	-0-	10'	10'
<u>All Other Streets</u>				
Front	15'	15'	15'	15'
Exterior Side (Corner Parcels)	15'	15'	15'	15'
Interior Sides	5'	10'	10'	10'
Rear	-0-	-0-	10'	10'

Note: All setbacks are measured from the property line.



**G. FEDERAL AVIATION ADMINISTRATION'S REGULATIONS ON CLEAR ZONES/BUILDING HEIGHTS**

Because of the close proximity of the project to Ontario International Airport, building heights will be restricted as required under the Revised Part 77 of the Federal Aviation Administration (FAA) Regulations. These regulations have been incorporated in the Los Angeles Department of Airports drawing 781000-800, dated May 24, 1978. (Note: This is summarized in EIR 81-4 Addendum Exhibit 13.) Notification of the FAA will be required for any structure proposed within the California Commerce Center that exceeds these restrictions.

A trapezoidal clear zone (2500 feet in length) has been established, consistent with the approved Part 150 study, for an area extending eastward from 200 feet beyond the 2200 feet dual runway extension. No structures are to be constructed within this zone.

~~California Commerce Center has offered to enter into an agreement that would dedicate approximately thirty (30) acres of land adjacent to the existing Ontario International Airport (ONT) Clear Zone. The offer is an effort to encourage the Los Angeles Department of Airports to implement the runway extension program adopted for Ontario International Airport by the Board of Airport Commissioners' Noise Abatement Policy Resolution No. 13513.~~

~~The final boundary of Clear Zone will be contingent upon a determination by the pending FAR Part 150 study relative to possible runway expansion to the east. The disposition of said thirty acre airport reservation area shall be subject to the adoption and approval of the City of Ontario and the Ontario Airport once the Part 150 study is completed.~~

~~Should the Part 150 study determine that a runway expansion is not necessary, the 30 acres will no longer be subject for airport reservation use and shall be designated as follows: 7.3 acres will remain in the clear zone, 14.4 acres will be designated as low employee intensity use, and 8.3 acres will no longer have development restrictions.~~

~~Exhibit 50, Clear Zone, Ontario International Airport Expansion, shows the location and configuration of the thirty acres to be dedicated. The specific terms of the dedication agreement will be negotiated with the City of Ontario and the Los Angeles Department of Airports, and confirmed in a duly executed three-party agreement.~~

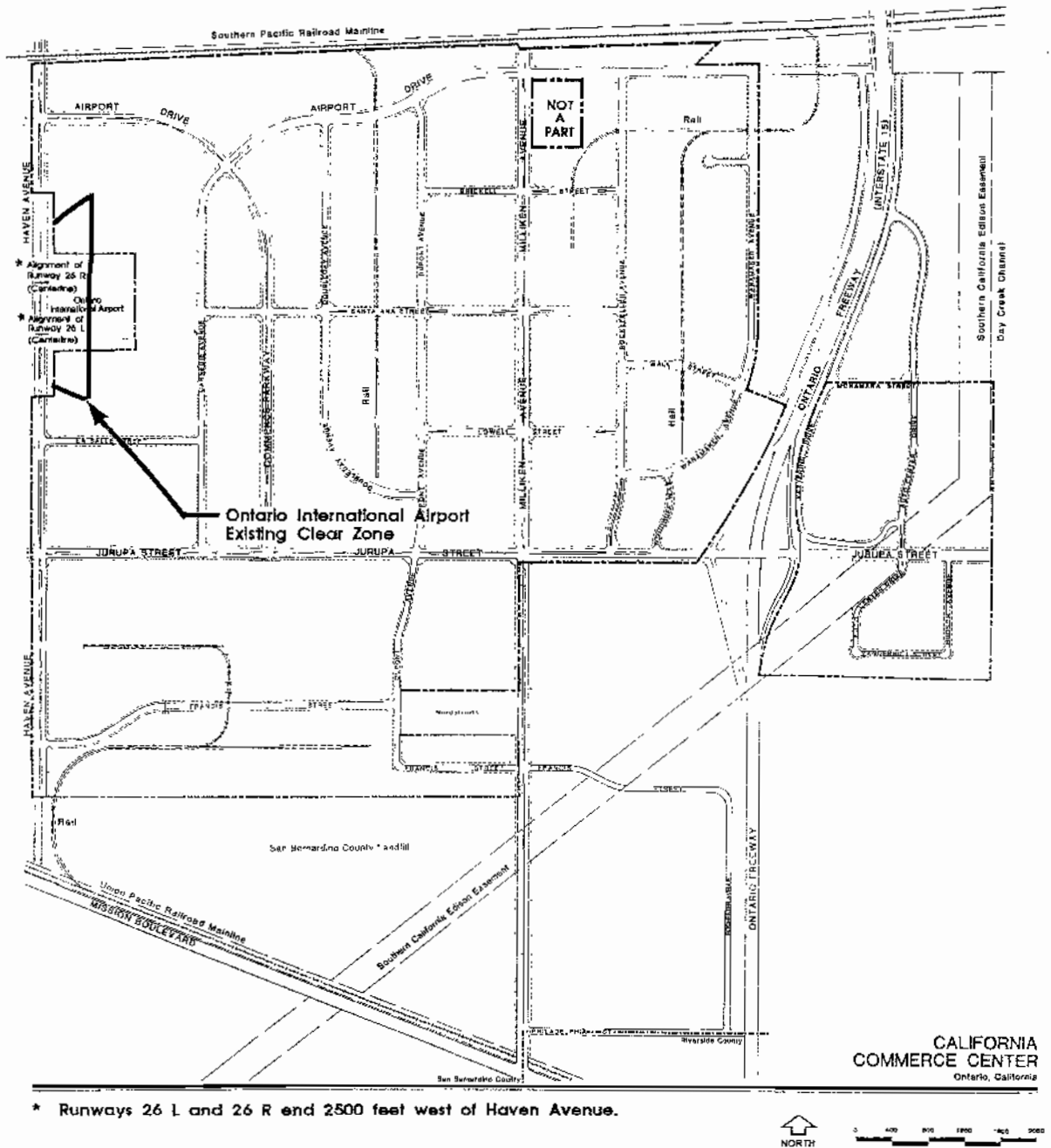
In addition, as part of FAA Regulations, a building height limitation of 200 feet above the ground elevation of the site will be imposed for all buildings in the project. The FAA also requires notification if any proposed structure will pierce an imaginary slope of 50' to 1' extending 2000 feet from the nearest point of the runway. Exhibit 51, Building Height Restrictions, New Runway and Existing Runway Extended 2200 Feet, plots these height restrictions as they relate to development within the project. Site Plans will be evaluated as to their conformance with these regulations.

## **H. SOUND ATTENUATION CRITERIA**

The project's location, adjacent to the Ontario International Airport, the Ontario Freeway, and the mainlines of Southern Pacific and Union Pacific Railroads, requires the implementation of sound attenuation measures for interior spaces. Maximum interior sound level criteria have been established for each land use. Exhibit 52 illustrates the maximum permitted interior noise levels (measured in  $LEQ_{12}$ ) for non-residential and residential construction (see Exhibit 53, Existing 1981 Noise Contours (CNEL) and Exhibit 54, CNELS for Airport Dual Runway).

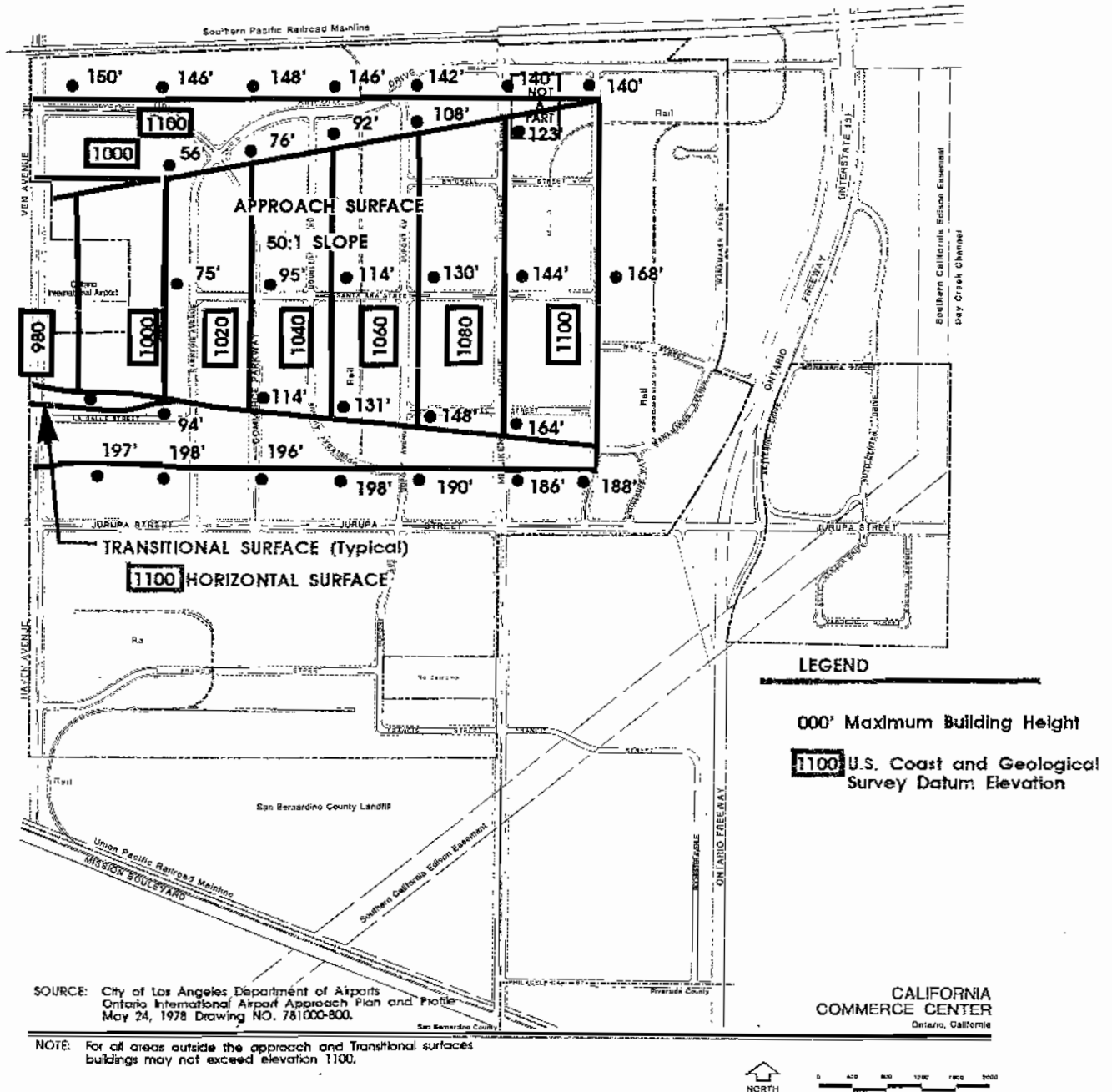
# EXHIBIT 50

## CLEAR ZONE Ontario International Airport Expansion



# EXHIBIT 51

## BUILDING HEIGHT RESTRICTIONS (New Runway and Existing Runway Extended 2200 Feet)



## MAXIMUM INTERIOR NOISE LEVELS, NON-RESIDENTIAL CONSTRUCTION

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### Noise Level Criteria for Hotel/Motel Construction

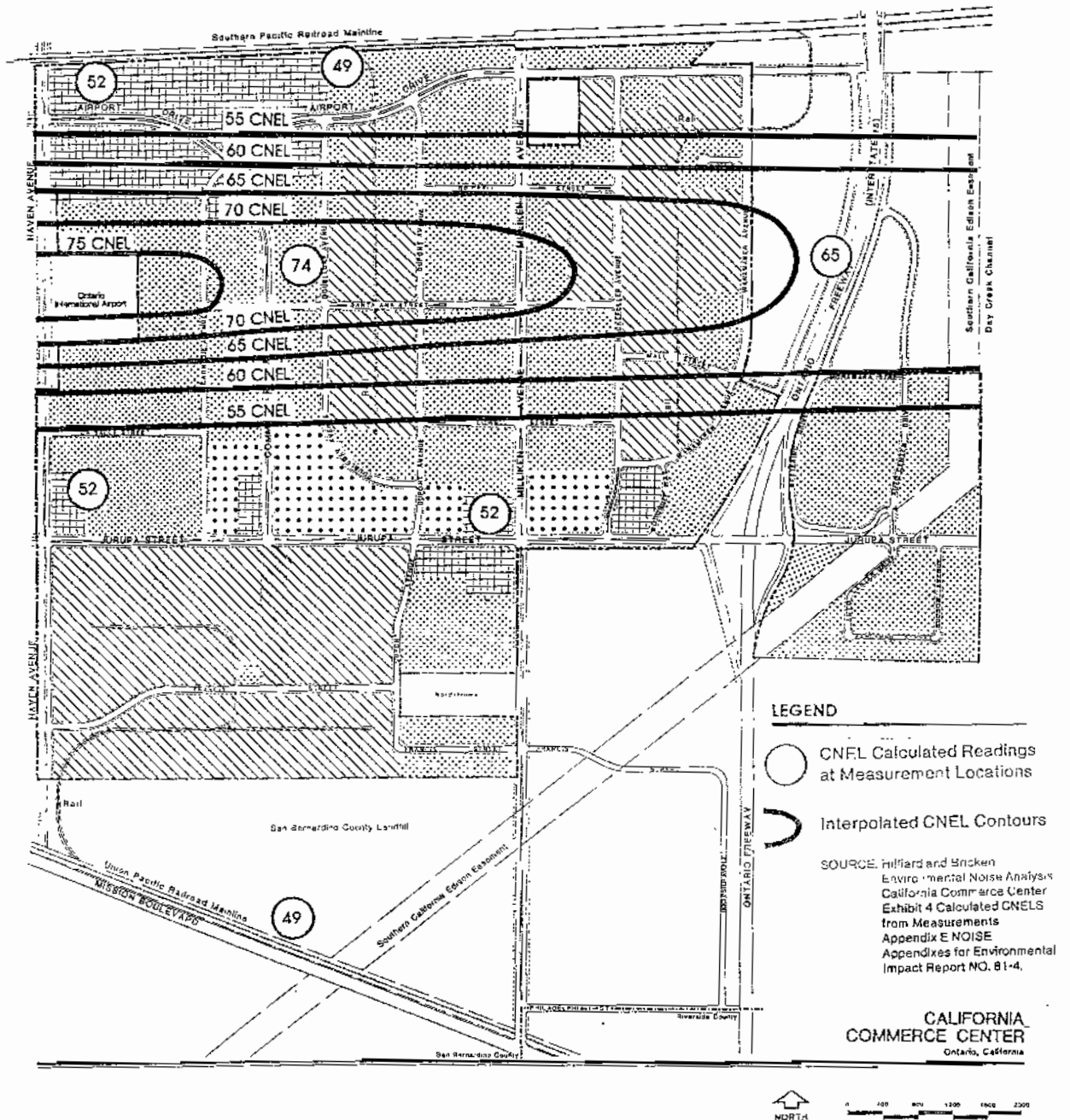
Noise levels during the hours from 7:00 a.m. to 7:00 p.m. which shall not be exceeded for the interior industrial/office/commercial spaces are as follows:

USE	SOUND LEVEL, $LEQ_{12}$
Private Offices	40-50 dBA
General Offices, Reception,	45-55
Typing, Clerical, Banks, Retail Stores	50-55
Other Uses and Areas for Manufacturing, Assembly Testing, etc	55-65

Where  $LEQ_{12}$  is the Energy Equivalent Sound Level during the hours 7:00 a.m. to 7:00 p.m.

# EXHIBIT 53

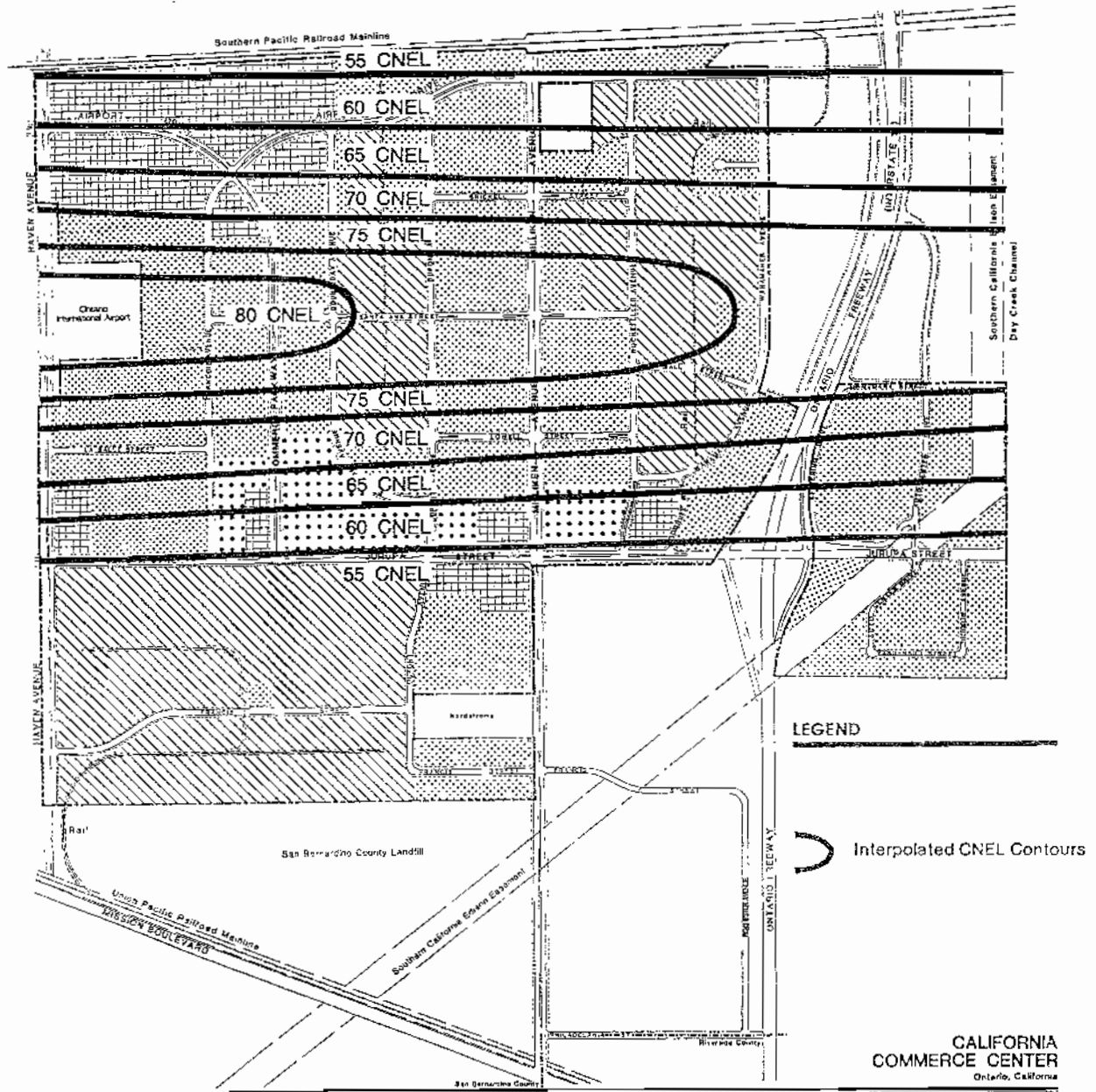
## EXISTING 1981 NOISE CONTOURS (CNEL)



NOTE: This exhibit for CNEL's only. For current land use, see exhibit 17.

# EXHIBIT 54

## CNEL'S FOR AIRPORT DUAL RUNWAY (New Runway and Existing Runway Extended 2200 Feet)

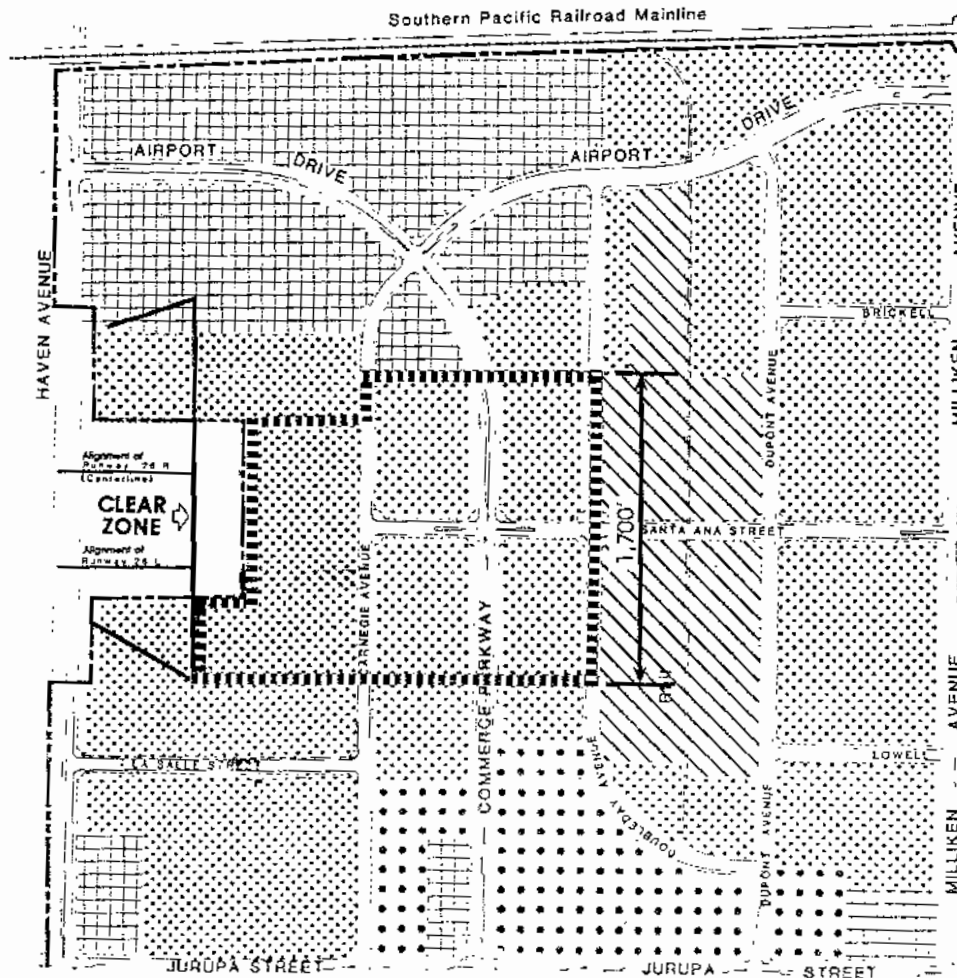


SOURCE: Ontario International Airport  
Final Environmental Impact Statement;  
Olsen Laboratories, Inc. August, 1975.  
Figure 2-7, CNEL Noise Exposure for  
Commercial Operations on the side-  
by-side runways, page 2-29.

NOTE: This exhibit for CNEL's only. For current land use, see exhibit 17.

# EXHIBIT 55

## AIRPORT RESTRICTIVE OVERLAYS (Existing Clear Zone, Low Employee Intensity Area)



This represents only a portion of the proposed project.

### LEGEND

- Existing Clear Zone
- ▨ Low Employee Intensity Area

\* Runways 26L and 26R end 2500 feet west of Haven Avenue.



## **I. CRITERIA FOR DEVELOPING ADJACENT TO FREEWAYS**

The City of Ontario has established standards for regulating development adjacent to Mission Boulevard, the San Bernardino Freeway (I-10), the Pomona Freeway (SR 60), and the Ontario Freeway (I-15). These standards have been adopted by the Ontario Planning Commission, Resolution No. 2392, May 27, 1980, and shall apply to such development within the California Commerce Center's project.

### **1. BUILDING ORIENTATION**

- a. All buildings shall face the highway, except where the highway is substantially elevated.
- b. The size, height, number, and type of on-premise signs shall be the minimum necessary for identification pursuant to the California Commerce Center's Master Signage Plan.
- c. Open storage of materials and equipment should be permitted only when incidental to the permitted use, provided that such storage area shall not face the highway, and shall be shown and approved on the site plan.
- d. Overhead doors, garages or loading zones shall be placed facing away from view of the highway.
- e. All mechanical equipment shall be screened from public view.

### **2. LANDSCAPING**

- a. Not less than 20 feet of landscaping, measured from the public right-of-way, shall be provided and permanently maintained.
- b. Proposed development should be designed to preserve existing stands of trees wherever practicable.

## **J. AIRPORT RESTRICTIVE OVERLAYS**

In order to reduce the possibility of serious aircraft-related incidents occurring on-site, a concept has been developed to limit the types of land uses permitted beneath the flight path of the Ontario International Airport.

### **1. PERMITTED USES**

#### **a. Clear Zone/Airport Reservation Area**

Within the boundaries of the Clear Zone/Airport Reservation Area, except as stated in subsection "G" page 147, no permanent structures shall be permitted (as per Mitigation Measure #36 of the EIR and the Specific Plan). Employment shall not exceed ten (10) persons/acre.

The following uses shall be permitted:

- Small aircraft storage and taxiways
- Auto rental agencies
- Auto storage for rental agencies or other auto related businesses located elsewhere (excluding auto dismantling businesses)
- Pay parking lots (excluding parking structures)
- ALUC review is required for any modification from the adopted Specific Plan amendment (3616-SPA) approved by the ALUC on November 4, 1987 for areas within the Clear Zone
- Land Use changes within the Clear Zone/Airport Reservation Area shall comply with the recommendations of the Los Angeles Department of Airports regarding the proposed specific land uses and the effect they may have on aircraft operations and future airport operations
- Employees shall not be stationed in the Clear Zone for prolonged periods of time

b. Low Employee Intensity Area

Within the boundaries of "Low Employee Intensity" area, as indicated on Exhibit 55, employment shall not exceed twenty-five (25) persons per acre or no more than 150 people in one building, whichever is less. Only one building may be permitted per site with a maximum lot coverage of 50%. The following uses shall be permitted within the Low Employee Intensity area, and the Light Industrial Development standards shall apply:

- Manufacturing, assembly, and testing uses, the operation of which is automated with limited number of personnel
- Warehousing of goods for distribution
- Storage uses including self-storage which requires a limited number of caretaker personnel
- Aircraft storage and taxiways
- Auto rental agencies
- Auto storage for rental agencies or other auto related businesses located elsewhere (excluding auto dismantling businesses)
- Pay parking lots
- Printing and lithography
- Administrative and business offices incidental to permitted uses
- Other uses which demonstrate that the nature of their operation does not require a large concentration of personnel, subject to review and approval by the City Planner. In the event of disagreement, approval of the proposed uses shall be determined by the Planning Commission

2. PROHIBITED USES

- Restaurants
- Retail
- Hospitals
- Rest/Retirement Homes

- High Intensity Public Assembly
- Landfills
- Residential
- Theaters
- Bowling Alleys
- Motels/Hotels
- Schools
- Libraries
- Governmental Institutions
- Indoor/Outdoor Recreation
- Petroleum Related Industry
- Smoke emitting uses that may endanger aircraft operations
- Uses which could emit electronic interference that could endanger aircraft operations. Such proposed uses may be required to obtain a determination of compatibility from the FAA and Ontario Airport

### 3. DEVELOPMENT STANDARDS

- a. Proposed developments should be designed with no structures directly on the extended runway centerline. Parking lots can be located in line with runway approach.
- b. All parking lots (including pay and storage lots) shall be developed subject to the development standards contained in this Specific Plan.
- c. All exterior illumination and parking lot lights shall be designed so that they do not cause skyward glare which may endanger aircraft operations.
- d. Buildings within this area shall install aircraft warning lights on the roof as approved by the FAA.
- e. All development shall be constructed in conformance with Federal Aviation Regulations (FAR) Part 77.
- f. A noise assessment shall be required for all development within the Airport Restrictive Overlay Area. Mitigation Measures shall be applied as conditions of approval to reduce interior noise levels below 55 dba (LEQ<sub>12</sub> per Exhibit 52).
- g. No structure shall be erected or object be placed or allowed to grow, which would protrude into the imaginary surfaces as established by Federal Aviation Regulations Part 77.
- h. An Aviation and Noise Easement shall be granted to the Ontario International Airport and recorded prior to the issuance of any permits for site development and a copy forwarded to the City of Ontario and to the affected airport.

#### 4. REQUIRED SUBMITTALS

The applicant for a use within the Clear Zone and Low Employee Intensity area shall be required to submit the following information for review and approval by the City Planner prior to issuance of a business license:

- A written description of the nature of the anticipated use, with sufficient information to enable the City Planner to determine the compatibility of the use in this area. The description shall include but not be limited to information regarding operations, number of employees on a maximum shift, number of shifts in any day, customer service (including number of customers who must come to the site), hours of operation, etc.
- Pictures of the same or similar use, if any, which is in existence elsewhere.
- Any other additional material which the City Planner might ask for to clarify any aspects about the use.

#### K. COMMUNITY FACILITIES -- POLICE PROTECTION

Commercial developments within the project area may require the use of on-site security, and/or Ontario Police department facilities. If on-site security is required, the applicant will have the option of providing in-house security, or contracting with an outside security company. Whatever security system is chosen, it must meet with the approval of the Ontario Police Department pursuant to OMC 3-1601.

The use of physical security measures, i.e. CCTV, Card Access, Burglar and Robbery Alarms, as well as other electronic security measures, will be utilized as necessary to provide adequate surveillance of the site and security for persons and property at the site.

The projected fiscal impact to the Ontario Police Department may be mitigated by the payment of a one-time developer impact assessment fee according to a schedule of fees contained in a pending Development Assessment Fee Ordinance that is yet to be adopted by the Ontario City Council.

In addition to the payment of Impact Fees, the developer must also comply with all physical security requirements contained in the Ontario Security Code, OMC 4-11.01.